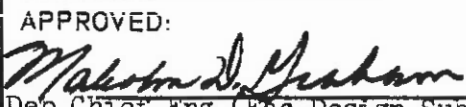
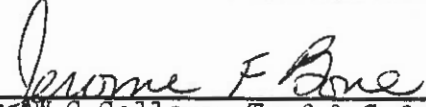


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TO: Director, Preliminary Plan Review Bureau SUPERSEDED BY EB 00-009 EFFECTIVE 1/24/2000	ENGINEERING INSTRUCTION NEW YORK STATE DEPARTMENT OF TRANSPORTATION
Distribution: <input checked="" type="checkbox"/> Main Office <input checked="" type="checkbox"/> Regions <input type="checkbox"/> Special	Code: <u>76-74</u>
APPROVED:  Dep. Chief Eng. (Fac. Design Subdiv.)	Date: <u>12/27/76</u> Supersedes:
 W.G. Galloway, Traf. & Safety Div.	

The purpose of this Instruction is to clarify Section 290.11, Location of Vehicular Signal Faces, of the New York State Manual of Uniform Traffic Control Devices when used on both federal-aid or 100% State contracts.

In accordance with Federal Highway Administration Ruling Sg-3-71 concerning interpretation of subsections 4 and 5 of section 4B-12 of the National Manual on Uniform Traffic Control Devices, subsections (f) (1) (iv) and (f) (2) (i) of section 290.11 of the New York State Manual of Uniform Traffic Control Devices are to be applied in combination. The intent is that at least one of the signal faces both meet the longitudinal distance requirement of subsection (f) (1) (iv) and fall within the cone described in subsection (f) (2) (i).

This Instruction is effective immediately.

On all future PS&E submissions, the above requirements will be enforced.

MT:JM:MAK

