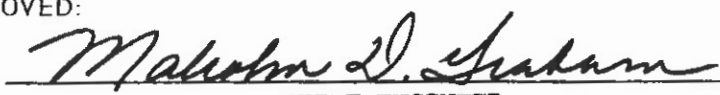


TO:  Director, Preliminary Plan Review Bureau  <b>MODIFIED BY EI 74-090</b> <b>EFFECTIVE 9/24/1974</b>	<h1>ENGINEERING INSTRUCTION</h1> <p>NEW YORK STATE DEPARTMENT OF TRANSPORTATION</p>
Distribution: <input checked="" type="checkbox"/> Main Office <input checked="" type="checkbox"/> Regions <input type="checkbox"/> Special	Code: <u>  EI 74-84  </u>
APPROVED:   _____ CHIEF ENGINEER	Date: <u>  8/29/74  </u>  Supersedes:

As a result of a review of the subject problem with FHWA and the Engineering Research and Development Bureau, it has been determined that the policy to permit guiderail placement within the limits of, less than one foot or greater than five feet from the face of curb, should be rescinded.

Effective on receipt of this memorandum, no plans shall be submitted for review or letting wherein curbing over three inches in height is within the critical limits; greater than one foot and less than 10 feet, of any guiderail or barrier system on highways with a design, posted, or operating speed in excess of 50 mph. In addition, no curbing shall be shown from the approach end of the guiderail or barrier back on a 15 degree angle from the edge of pavement. A revision to the Design Manual incorporating this change will be made available shortly.

The three inch height of the curbing is to be measured from the pavement surface at the face of the curb. The curb shall be set to the proper height without consideration of future resurfacing.

Existing curbing standards with respect to Jersey median barrier remain in effect.

MDG:JEH:AV

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